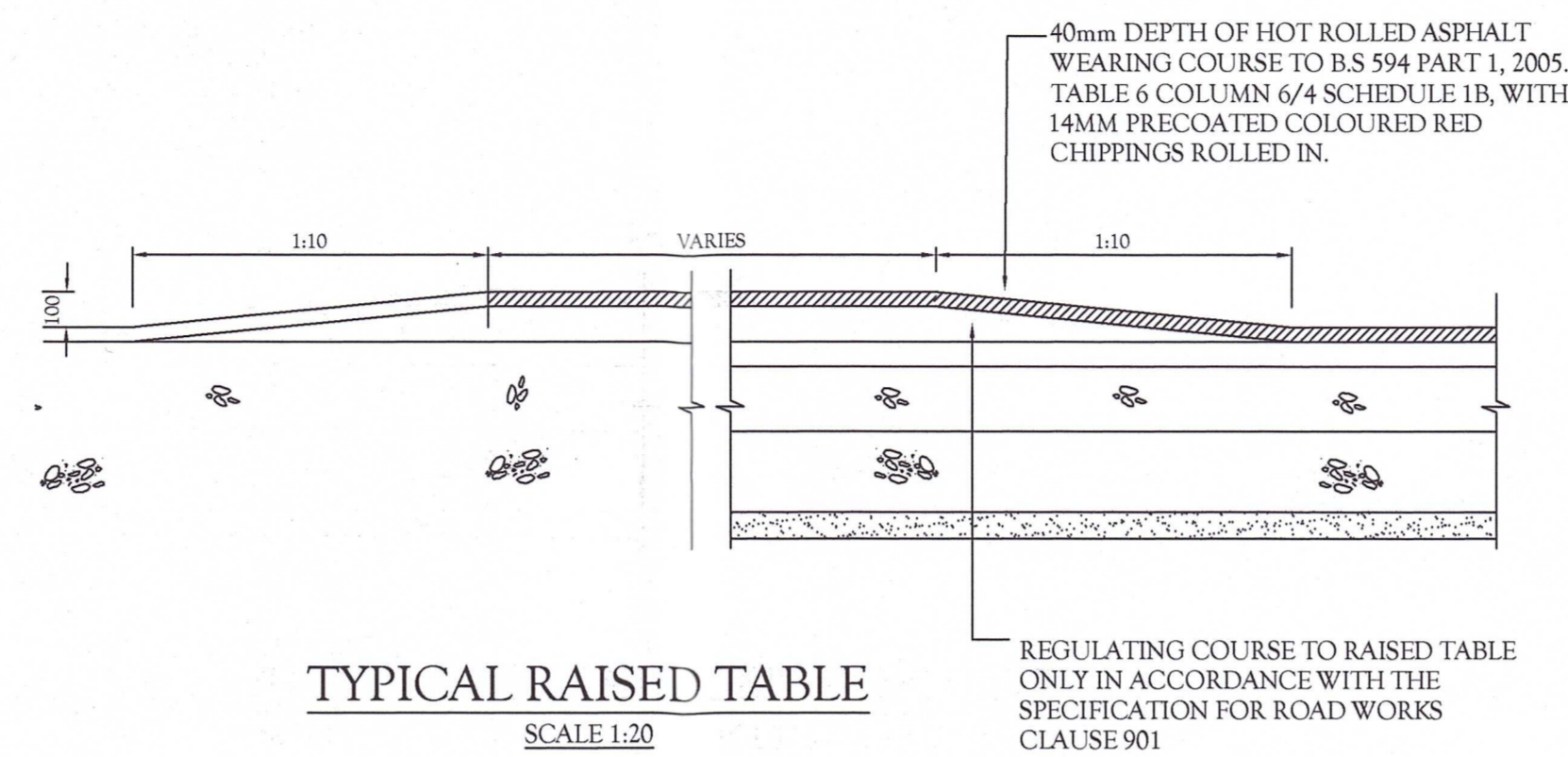
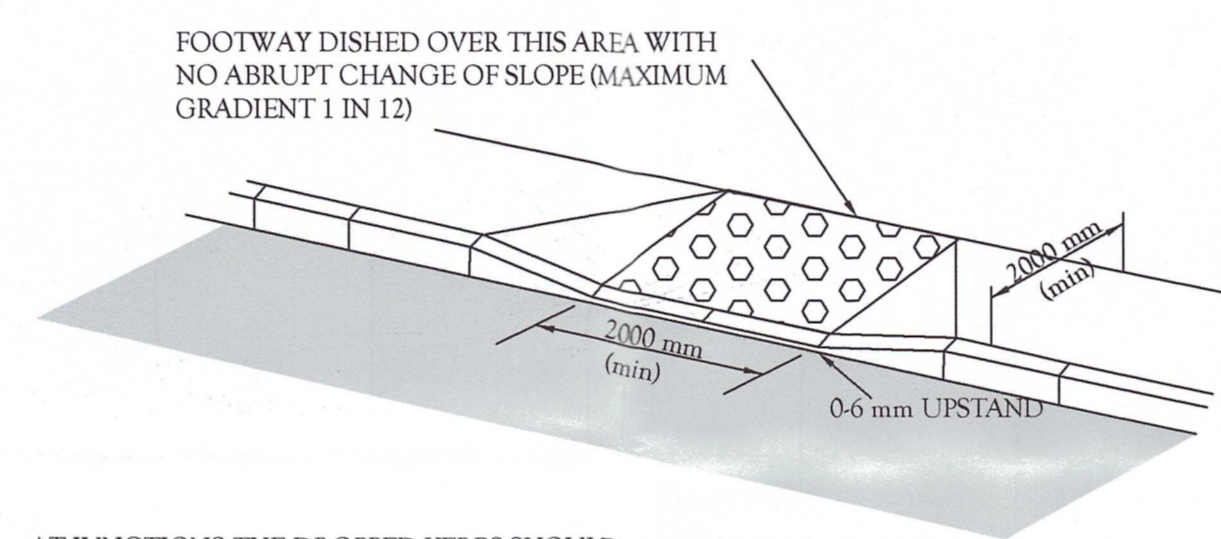


DETAILED CONSTRUCTION OF NEW ROAD & FOOTPATH
SCALE 1:20

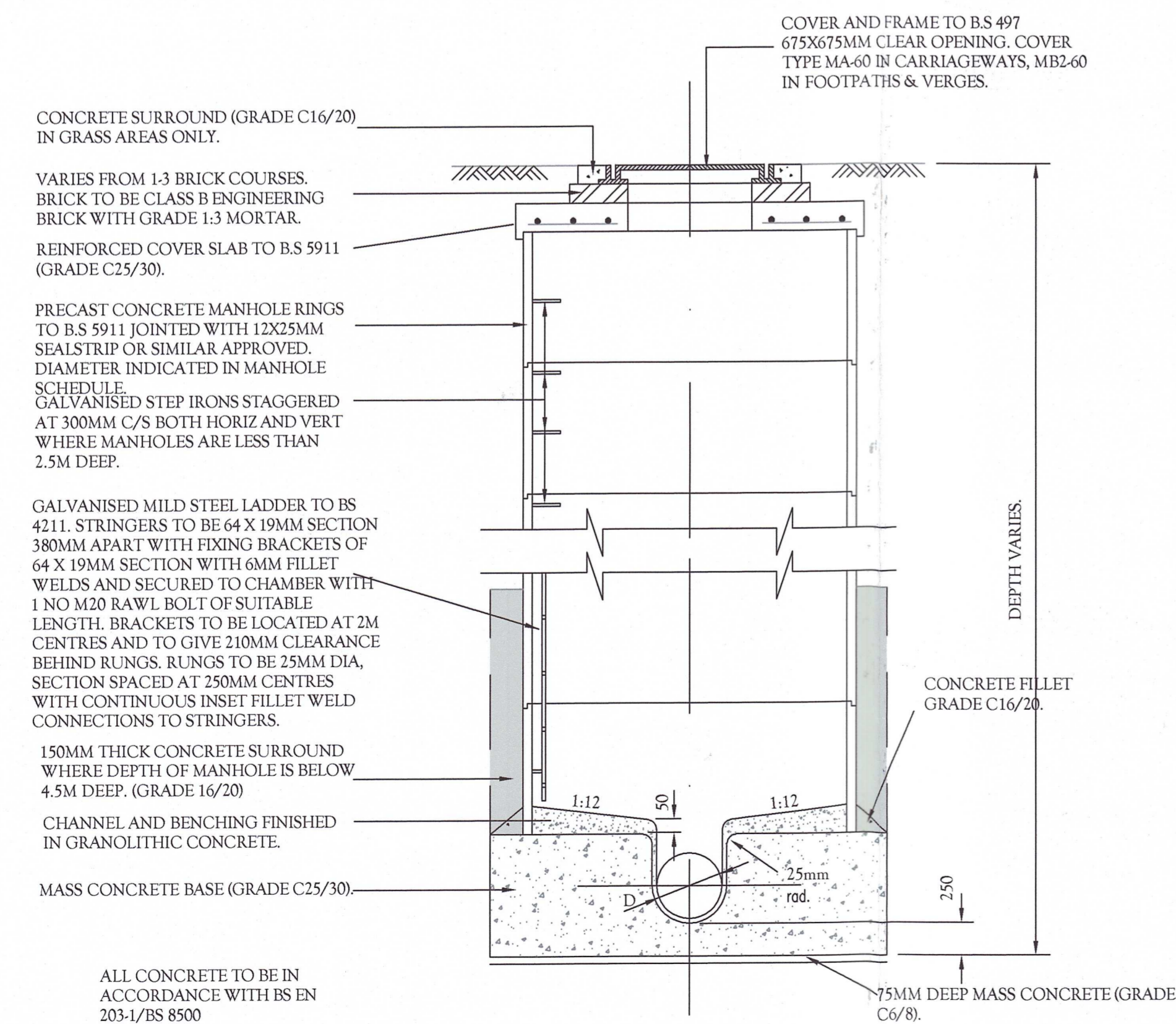


TYPICAL RAISED TABLE
SCALE 1:20

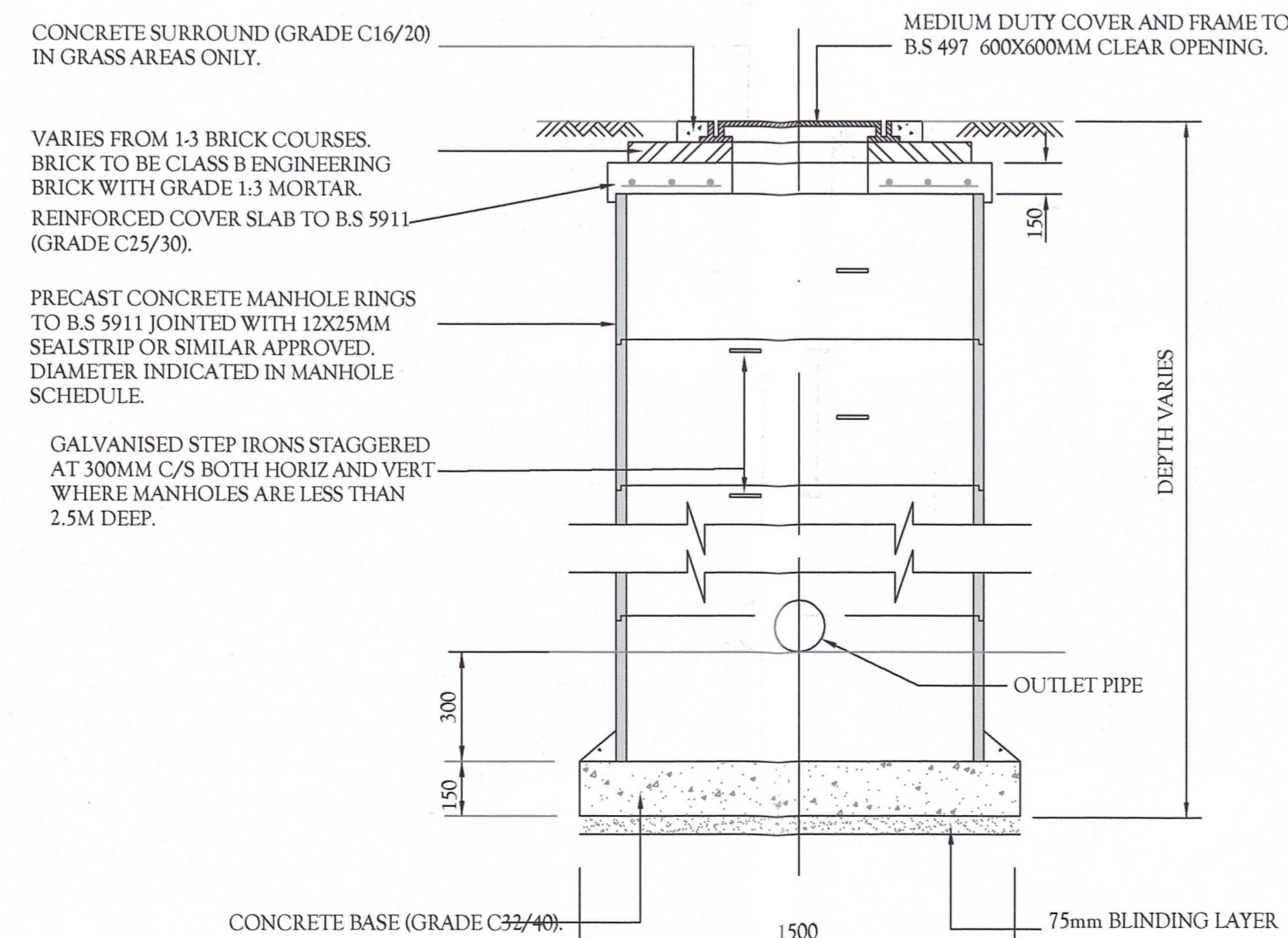


AT JUNCTIONS THE DROPPED KERBS SHOULD BE LOCATED AT THE END OF THE ENTRY RADIUS ON THE MINOR ROAD.

PEDESTRIAN CROSSING POINT
SCALE 1:100



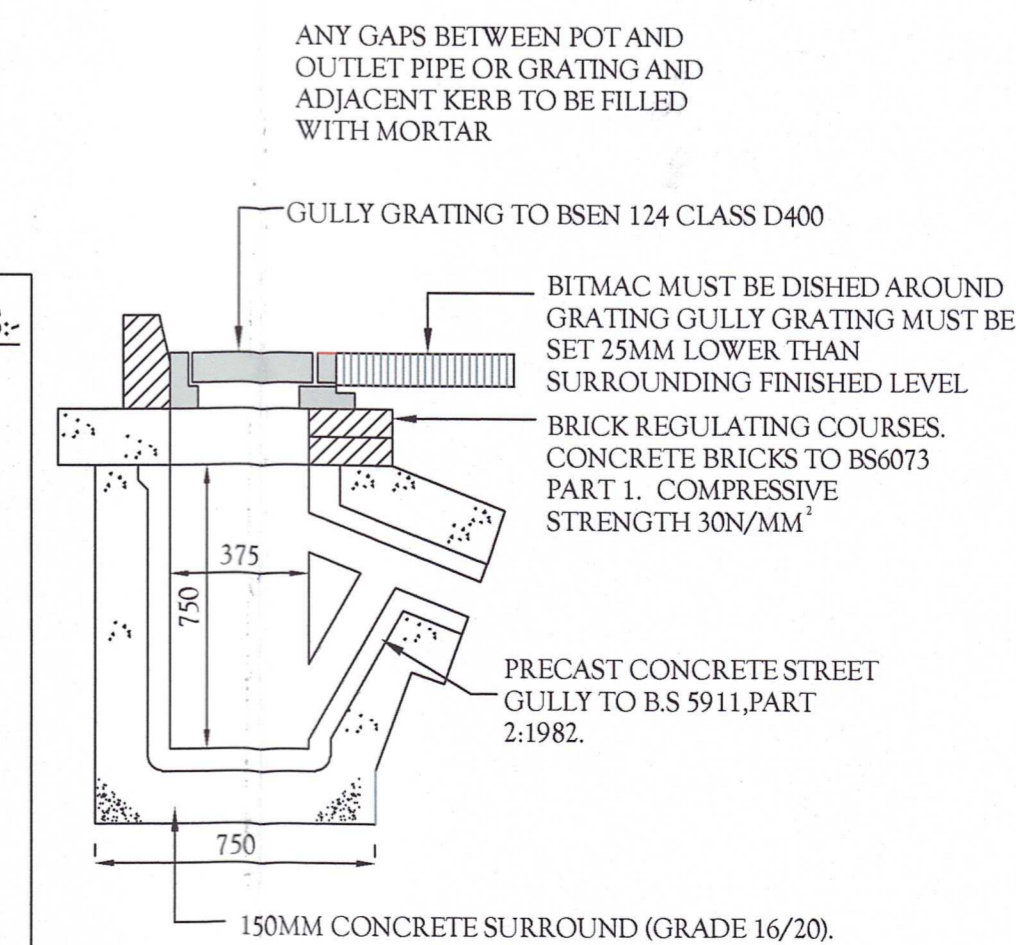
DETAIL OF PRECAST CONCRETE MANHOLE
SCALE 1:20



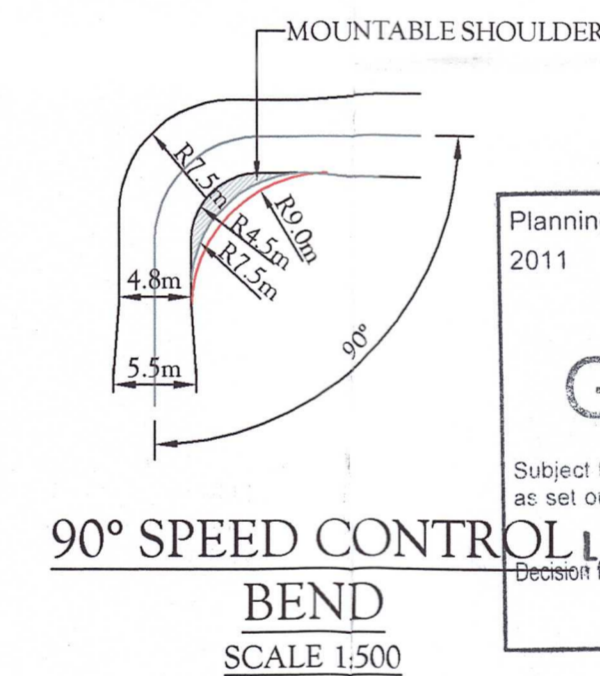
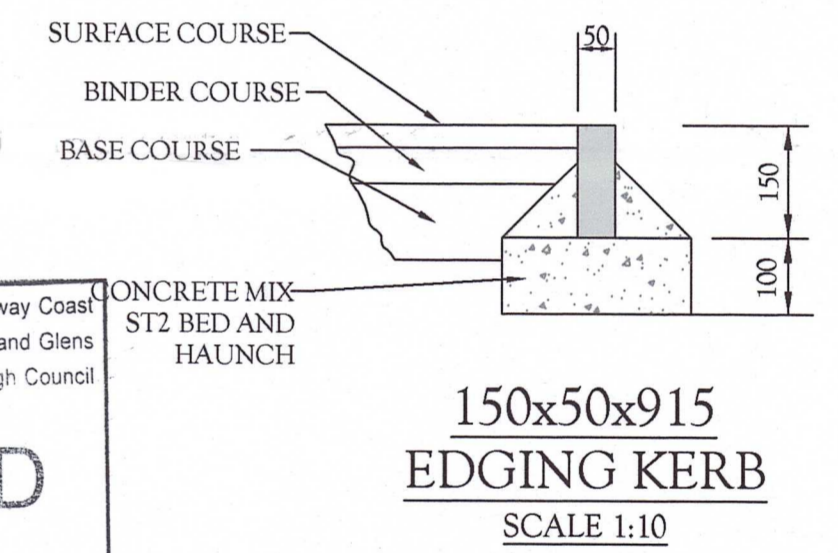
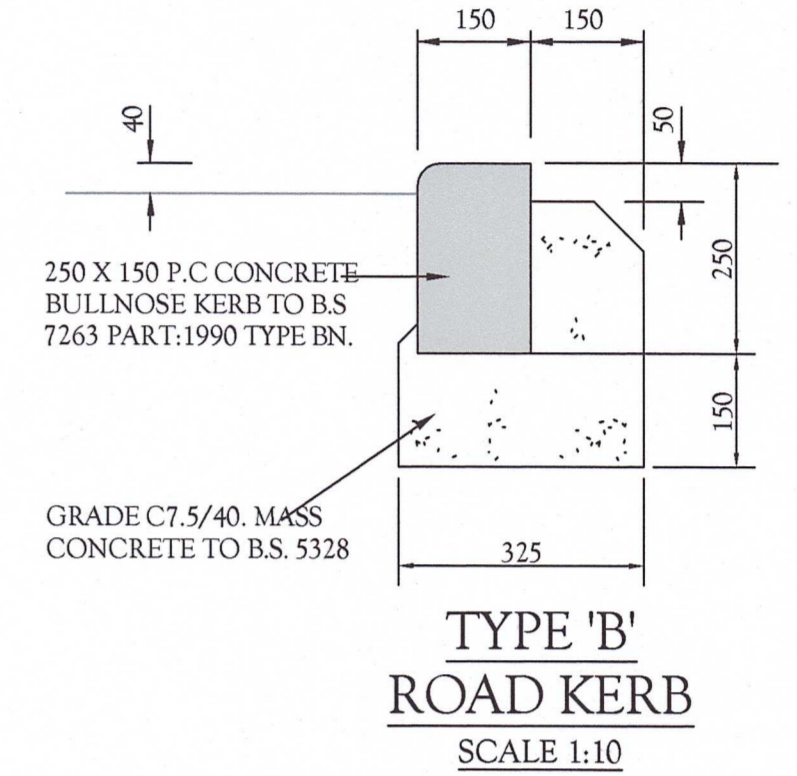
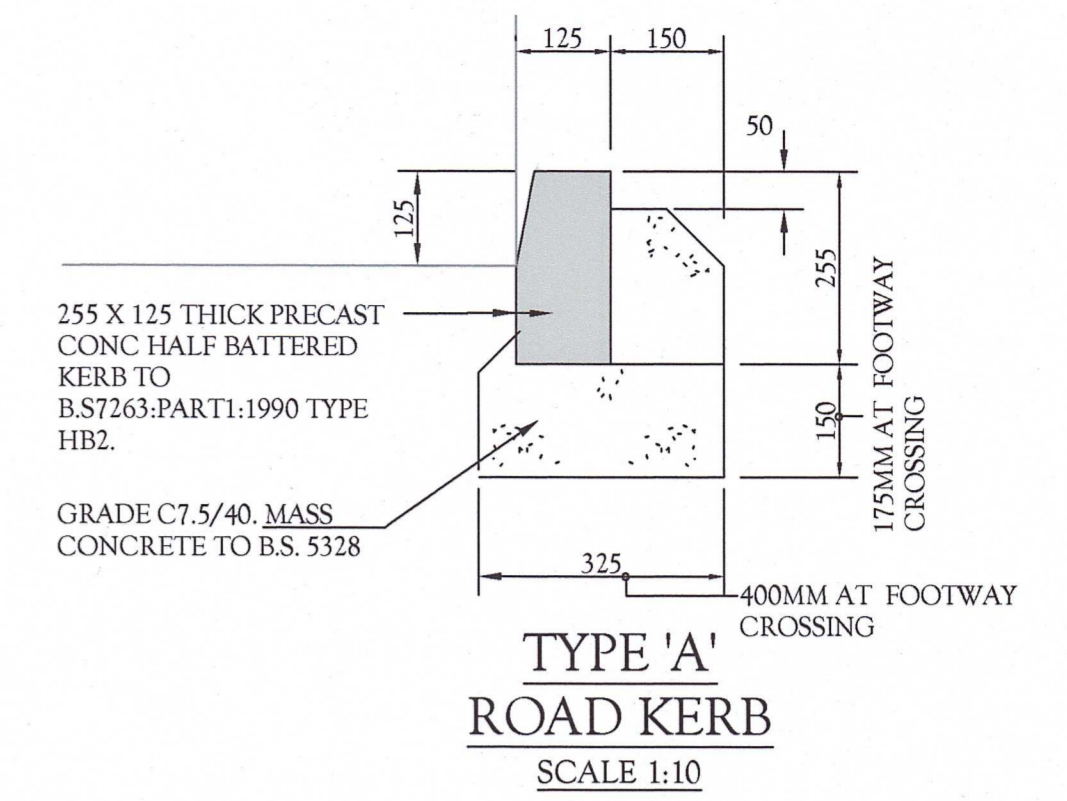
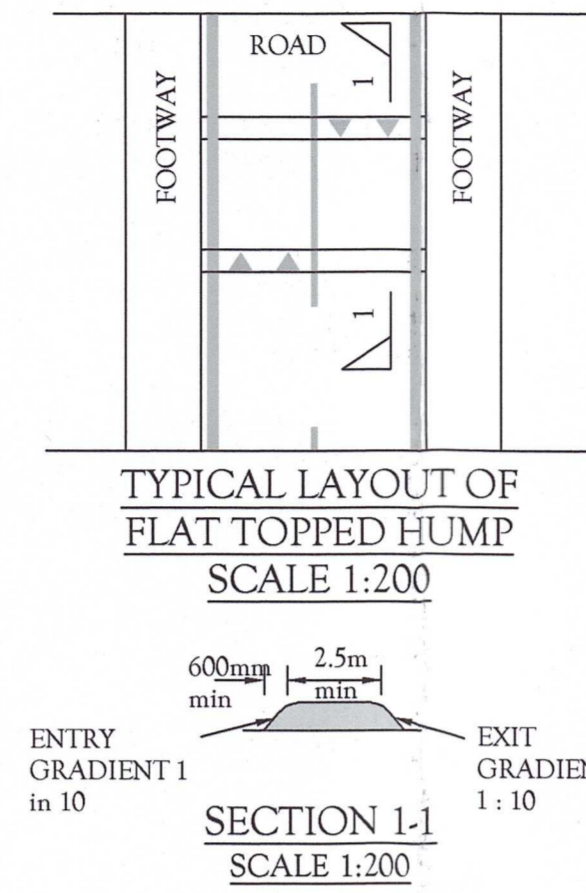
TYPICAL SILT PIT DETAIL
SCALE 1:20

CONCRETE ROAD GULLY NOTES:-

1. THE GULLY GRATING AND FRAME SHOULD BE IN ACCORDANCE WITH BS 497 PART 1 1976 PARAGRAPH 13. HEAVY DUTY AS APPROPRIATE (SEE 5.12.8(D)).
2. GULLY GRATING SHOULD BE SET TO 10MM BELOW CARRIAGEWAY PROFILE WHERE APPROPRIATE, ON A FULL MORTAR BED OF 1:3 CEMENT/SAND MORTAR.
3. BRICKWORK SHOULD COMPLY WITH BS 3921 FOR CLASS B ENGINEERING BRICKS IN TWO COURSES SET ON 1:3 CEMENT/MORTAR WITH WEEPHOLES FORMED IN THE FIRST COURSE.
4. CARRIAGEWAY GULLY POTS SHOULD HAVE RODDING EYES AND STOPPERS TO BS 5911 PART 2 1982 AND BE 450 X 900MM DEEP.
5. GULLY POT SHOULD BE SURROUNDED WITH 150MM MIN. THICK CONCRETE CLASS 22.5/20.0.



TRAPPED GULLY DETAIL
SCALE 1:20



Planning Act 2011

Causeway Coast and Glens Borough Council

GRANTED

Subject to Conditions (if any) as set out on

Decision Form No. LA01/2019/0993

Date: 23/12/2019

LA01

2019/0993

Planning Office RECEIVED

20 OCT 2020

File No.

Causeway Coast and Glens Borough Council

Drawing Number ... CTA ...

TITLE BLOCK REVISED	A	15.08.19	MD	ROS
AMENDMENTS	REV	DATE	BY	APPR

SHEEHY CONSULTING

Consulting Civil & Structural Engineers, Project Managers
Environmental Engineers, CDM Co-ordinators & Traffic Engineers

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STATUS: **PLANNING**

CLIENT: **MCALISTER BUILDERS**

ARCHITECT: **VISION DESIGN ARCHITECTS**

PROJECT: **PROP. RESIDENTIAL DEVELOPMENT, MILL COTTAGE DRIVE, STRANOCUM**

TITLE: **CONSTRUCTION DETAILS**

SCALE: **AS SHOWN @ A1** DATE: **JUNE 2019**

DRAWN BY: **MD** CHECKED BY: **MC** APPROVED BY: **RCS**

PROJECT No.: **19-1056** DRAWING No.: **C05** REV. No.: **A**